

**NEW YORK STATE THRUWAY AUTHORITY
DEPARTMENT OF ENGINEERING**

**ALBANY, NY
MAY 10, 2024**

**AMENDMENT NO. 1
TO**

**CONTRACT TAS 24-20B
D214978
REPLACEMENT OF THE I-90 BRIDGE OVER
ORISKANY BOULEVARD (NY 69) AT MILEPOST 238.22
IN THE SYRACUSE DIVISION
OF THE NEW YORK STATE THRUWAY
IN ONEIDA COUNTY**

**IN THE LETTING OF
MAY 29, 2024**

NOTE: This amendment shall be attached to and become a part of the Proposal for Contract **TAS 24-20B**.

PLANS

1. **DELETE** Drawing GNN-2, Sheet 7, and **SUBSTITUTE** with the attached Drawing GNN-2, Sheet 7A1. Workzone Traffic Control Note 2 was revised to also require 14 days advance notice to the Mohawk Valley Transportation Management Center (MVTMC) prior to any lane closures on Oriskany Boulevard (NY 69).
2. **DELETE** Drawing TC-1, Sheet 14, and **SUBSTITUTE** with the attached Drawing TC-1, Sheet 14A1. Note 4 was revised to also require seven (7) calendar days advance notice to the Mohawk Valley Transportation Management Center (MVTMC) prior to any construction activities requiring the stopping of traffic on Oriskany Boulevard (NY 69).
3. **DELETE** Drawing ST-2, Sheet 61, and **SUBSTITUTE** with the attached Drawing ST-2, Sheet 61A1. The clear zone on the Proposed Bridge Elevation was revised from 15'-0" to 10'-0" and Note 1 was added detailing the determination of the clear zone by the NYSDOT.
4. **DELETE** Drawing ST-7, Sheet 66, and **SUBSTITUTE** with the attached Drawing ST-7, Sheet 66A1. The clear zone on the Section A-A Backfill was revised from 15'-0" to 10'-0", the 1:3 slope on Section A-A Backfill was removed, the 1:2 slope was extended to meet the slope behind the existing sidewalk, and Note 3 was added detailing the determination of the clear zone by the NYSDOT.
5. **DELETE** Drawing ST-49, Sheet 108, and **SUBSTITUTE** with the attached Drawing ST-49, Sheet 108A1. The sheet was revised to remove inapplicable tables and notes, more specifically, the Expansion Joint Opening Table, Notes 8 through 12, and the reference to Note 9 in the Sleeper Slab Detail.
6. **DELETE** Drawing ST-53, Sheet 112, and **SUBSTITUTE** with the attached Drawing ST-53, Sheet 112A1. The Barrier End View was revised to include the bearing plate on the back side of the barrier shown and labeled in the plan view.

The Bidder **MUST** complete Page **197** of the Proposal acknowledging receipt of this amendment. If the Bidder fails to complete the “Amendment Acknowledgement” sheet, his bid could be declared informal thereby delaying award of the contract.

PLEASE BE GOVERNED ACCORDINGLY WHEN SUBMITTING BIDS.

Brent E. Howard, P.E.
Chief Engineer

SUPERSTRUCTURE NOTES:

- 8. THE DESIGN OF THE STRUCTURE ASSUMES THAT THE STRUCTURAL STEEL IS COMPLETELY ERECTED BEFORE IT IS ALLOWED TO DEFLECT UNDER ITS OWN (STEEL) DEAD LOAD. DEFLECTIONS INCURRED DURING THE VARIOUS STAGES OF THE ERECTION METHOD ARE NOT CONSIDERED. THEREFORE, THE ACTUAL ERECTION METHODS AND SEQUENCES EMPLOYED BY THE CONTRACTOR MAY HAVE SUBSTANTIAL EFFECT ON THE FINAL STEEL PROFILE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TAKING ALL NECESSARY COMPENSATORY ACTION TO ENSURE THAT THE FINAL ALIGNMENT AND PROFILE OF THE ERECTED STEEL CONFORMS TO THE NEW YORK STATE STEEL CONSTRUCTION MANUAL. ANY CORRECTIVE WORK NECESSARY TO RE-POSITION PREVIOUSLY ERECTED STEEL TO ACHIEVE THE ACCEPTABLE ALIGNMENT AND PROFILE MUST BE APPROVED BY THE ENGINEER, AND SHALL BE PERFORMED AT NO ADDITIONAL COST TO THE AUTHORITY. THE COST OF ERECTION SHALL BE INCLUDED IN THE PRICE BID FOR THE STRUCTURAL STEEL ITEM.
- 9. DIAPHRAGMS SHALL BE FABRICATED TO FIT GIRDERS ERECTED WITH THEIR WEBS PLUMB UNDER FULL DEAD LOAD CONDITIONS, ALSO KNOWN AS TOTAL DEAD LOAD FIT (TDLF).
- 10. GIRDER FINISH NOTE (METALIZING, ETC)

THE STRUCTURAL STEEL FOR THE BRIDGE SHALL BE METALIZED TO THE LIMITS NOTED AND DEPICTED ON THE GIRDER ELEVATION DRAWING. SURFACE PREPARATION AND APPLICATION SHALL BE PERFORMED IN ACCORDANCE WITH, AND PAID FOR UNDER, ITEM 572.00020125 - METALIZING, TYPE 1. THE CONTRACTORS ATTENTION IS DIRECTED TO MATERIALS SECTION (D) OF THE METALIZING SPECIFICATIONS WHICH PROVIDES CONTRACTOR SUBMITTAL OF THE METALIZING SPECIFICATION REQUIREMENTS, WHICH MUST BE SUBMITTED AND APPROVED PRIOR TO THE COMMENCEMENT OF ANY SHOP METALIZING. ADDITIONALLY, THE AREAS THAT ARE NOT TO BE METALIZED AND SEALED SHALL BE IDENTIFIED CLEARLY ON THE SHOP DRAWINGS (I.E. CONNECTION FAYING SURFACE, PORTIONS OF THE GIRDER TOP FLANGE).

METALIZING NOTES:

- a. ALL GIRDERS, INCLUDING BEARING STIFFENERS AND CONNECTION PLATES, SHALL BE METALIZED FOR THEIR ENTIRE LENGTH AND THE METALIZING SHALL BE PAID FOR UNDER ITEM 572.00020125 - METALIZING, TYPE 1.
- b. THERE SHALL BE NO WELDING TO METALIZED SURFACES.
- c. SHEAR STUD CONNECTORS SHALL BE WELDED PRIOR TO METALIZING. THE CONTRACTOR'S WORKER SAFETY PLAN SHALL SPECIFY THE TYPE OF WALKING/WORKING SURFACE TO BE USED SO THAT WORKERS DO NOT WALK ON ANY SURFACE WITH INSTALLED SHEAR STUD CONNECTORS.
- d. DRILLED HOLES SHALL BE CLEANED OF EXCESS GALVANIZED COATING THAT PREVENTS PROPER BOLT INSTALLATION.

11. OTHER STEEL ELEMENT PROTECTIVE FINISH NOTE

GALVANIZING NOTES:

- a. ALL BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.
- b. ALL EXPOSED STEEL SURFACES ON DIAPHRAGMS, UTILITY SUPPORTS, AND SHIMS SHALL BE GALVANIZED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.
- c. REASONABLE ACCOMMODATIONS FOR THE PREVENTION OF WET STORAGE STAINING (WHITE RUST) OF HOT-DIPPED GALVANIZED (HDG) MATERIALS SHALL BE PROVIDED AT ALL TIMES. STORAGE OF HDG MATERIALS OUTDOORS SHOULD BE AVOIDED IF POSSIBLE. IF OUTDOOR STORAGE IS UNAVOIDABLE, EXAMPLES OF REASONABLE ACCOMMODATIONS ARE FOLLOWS: STORAGE MATERIALS OFF OF THE GROUND AWAY FROM ALL VEGETATION, NON-RESINOUS WOODEN SPACES TO ALLOW VENTILATION AND AVOID MOISTURE BUILD UP, INCLINE MEMEBERS TO ALLOW DRAINAGE, EXAMPLES OF NON-RESINOUS WOODS ARE, POPLAR ASH, AND SPRUCE. WHITE RUST THAT IS DETERMINED TO DETRIMENTAL TO THE INTENDED USE OF THE MEMBER HAS A NEGATIVE VISUAL IMPACT ON THE STRUCTURE SHALL BE REPAIRED IN ACCORDANCE WITH THE NYS STEEL CONSTRUCTION MANUAL. WHITE RUST THAT IS DETERMINED TO BE CAUSED BY IMPROPER STORAGE OR SHIPING OF HDG MATERIALS SHALL BE REPAIRED AT NO COST TO THE THRUWAY AUTHORITY.

- 12. FIELD CLEANING OF METALIZED STRUCTURAL STEEL: THE OUTSIDE SURFACE OF THE FASCIA GIRDERS SHALL BE POWER WASHED SO THAT ALL TRACES OF DIRT, GREASE, CONCRETE SPALLTER OR OTHER FOREIGN MATERIAL IS REMOVED AT THE COMPLETION OF THE BRIDGE CONSTRUCTION. THE PURPOSE OF THIS CLEANING IS TO RETURN THE FASCIA SURFACES TO THE CONDITION IN WHICH THEY LEFT THE FABRICATION SHOP. THE COST FOR THIS WORK SHALL BE INCLUDED UNDER THE VARIOUS ITMES OF THE CONTRACT.
- 13. THE COST FOR ALL BOLTS, NUTS, WASHERS, WELDS AND DIAPHRAGM CONNECTION SHIM PLATES SHALL BE INCLUDED IN THE PRICE BID FOR THE STRUCTURAL STEEL ITEM. NO SPECIAL OR DIRECT PAYMENT WILL BE MADE FOR THESE MATERIALS.

SUPERSTRUCTURE SLAB NOTES:

- 1. DECK FORMS SHALL BE STAY-IN-PLACE (SIP) FORMS BETWEEN ALL GIRDERS AND SHALL BE REMOVABLE FORMS FOR BOTH FASCIA OVERHANGS. SIP FORMS AND SUPPORTS SHALL CONFORM TO ASTM A653, COATING DESIGNATION G235. FABRICATION SHALL BE IN CONFORMANCE WITH ASTM A924.
- 2. IN ORDER TO PREVENT MOVEMENT OF THE BRIDGE OVERHANG BRACKET DURING DECK CONCRETE PLACEMENT, AS WELL AS TO PREVENT LATERAL DISTORTION OF THE GIRDER WEB, AN OVERHANG BRACKET THAT IS BRACED BY THE BOTTOM FLANGE SHALL BE USED.
- 3. THE SURFACES OF GIRDERS SHALL BE PROTECTED FROM DECK OVERHANG FORM SUPPORTS TO PREVENT DAMAGE TO THE METALIZED SURFACES.
- 4. ALL LONGITUDINAL AND TRANSVERSE TOP MAT DECK REINFORCING SHALL BE STAGGERED 1/2 THE SPACING ABOVE THE BOTTOM MAT DECK REINFORCING STEEL UNLESS OTHERWISE INDICATED IN THE PLANS.
- 5. THE DETAILS FOR THE BARRIER REINFORCEMENT ARE FOR THE SLIP -FORMED OR CAST-IN-PLACE OPTION ONLY. COST OF BARRIER AND ANCHORAGE REINFORCEMENT ORIGINATING IN THE SLAB SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE SINGLE SLOPE (HALF SECTION) CONCRETE BRIDGE BARRIER, ITEM 569.04.
- 6. PROTECTIVE SEALER, ITEM 559.01, SHALL BE APPLIED TO ALL EXPOSED CONCRETE SURFACES OF THE STRUCTURAL DECK SLAB, BARRIERS, APPROACH SLABS, AND EXPOSED TOP SURFACES OF SLEEPER SLABS. ONLY PENETRATING TYPE SEALER AS INDICATED IN THE SPECIFICATION SHALL BE USED.

WORK ZONE TRAFFIC CONTROL NOTES:

- 1. PROTECTION OF THE PUBLIC: CONTRACTOR SHALL MAINTAIN AND PROTECT THRUWAY TRAFFIC IN ACCORDANCE WITH SECTION 619, THE NYSIA ADDENDUM TO THE STANDARD SPECIFICATIONS, THE TRAFFIC CONTROL PLANS, THE MUTCD, AND THE NEW YORK STATE SUPPLEMENT TO THE MUTCD. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE REQUIREMENTS OF SECTION 107, LEGAL RELATIONS AND RESPONSIBILITY TO THE PUBLIC, OF THE CURRENT STANDARD SPECIFICATIONS.
- 2. THE CONTRACTOR SHALL NOTIFY AND COORDINATE WORK ZONE TRAFFIC CONTROL ALONG ORISKANY BLVD NY 69 A MINIMUM OF 14 DAYS IN ADVANCE OF ANY LANE CLOSURES WITH:

NEW YORK STATE POLICE TROOP D
ERIC KNAPP
ERIC.KNAPP@TROOPERS.NY.GOV
(315) 366-6096

ONEIDA COUNTY EMERGENCY SERVICES
DAN APPLER
DAPPLER@OCGOV.NET
(315) 765-2530

MOHAWK VALLEY TRANSPORTATION MANAGEMENT CENTER (MVTMC)
(315) 733-2111

- 2. FOR ADDITIONAL WORK ZONE TRAFFIC CONTROL NOTES, SEE DWG. TCN-1.

EROSION & SEDIMENT CONTROL NOTES:

- 1. EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 209 OF THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION SPECIFICATIONS AND STANDARD SHEETS. ADDITIONAL GUIDANCE AND SIZING CRITERIA CAN BE FOUND IN THE MOST CURRENT EDITION OF THE NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL (THE BLUE BOOK) PUBLISHED BY THE NYS DEPARTMENT OF ENVIRONMENTAL CONSERVATION. ADDITIONAL SOIL EROSION AND SEDIMENT CONTROL MEASURES MAY BE REQUIRED AS PER SECTION 107-12 OF THE STANDARD SPECIFICATIONS.
- 2. THE CONTRACTOR WILL BE REQUIRED TO PERFORM ALL CONSTRUCTION OPERATIONS IN A MANNER SO AS TO MINIMIZE SOIL EROSION AND ENSURE SEDIMENT CONTROL.

- 3. ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO PREVENT DIRECT OR INDIRECT CONTAMINATION OF ALL WATER BODIES (INCLUDING WETLANDS) BY SILT, SEDIMENT, FUELS, SOLVENTS, LUBRICANTS, EPOXY COATINGS, CONCRETE LEACHATE, OR ANY OTHER POLLUTANT ASSOCIATED CONSTRUCTION. DURING CONSTRUCTION, NO WET OR FRESH CONCRETE OR LEACHATE SHALL BE ALLOWED TO ESCAPE DIRECTLY OR INDIRECTLY INTO ANY GROUND SURFACES OR WATER BODIES (INCLUDING WETLANDS), NOR SHALL WASHINGS FROM CONCRETE TRUCKS, MIXERS, OR OTHER DEVICES BE ALLOWED TO ESCAPE DIRECTLY OR INDIRECTLY INTO ANY GROUND SURFACES OR WATER BODIES (INCLUDING WETLANDS). CONTRACTOR SHALL PROVIDE A CONCRETE WASHOUT PIT IN ACCORDANCE WITH STANDARD SPECIFICATIONS SHOWN WITHIN NYSDEC'S 2016 BLUE BOOK (SECTION 2 RESOURCE PLANNING). COST SHALL BE INCLUDED IN THE VARIOUS 209 ITEMS.
- 4. ANY DEBRIS OR EXCESS MATERIAL FROM CONSTRUCTION OF THIS PROJECT SHALL BE IMMEDIATELY AND COMPLETELY REMOVED FROM THE BED AND BANKS OF ALL WATER BODIES (INCLUDING WETLANDS) AND SHALL BE DISPOSED OF AWAY FROM WETLANDS, WATER COURSES, OR OTHER BODIES OF WATER.
- 5. ALL EXCAVATED MATERIAL SHALL BE DISPOSED OF AND BE PROTECTED SO THAT IT CAN NOT DIRECTLY OR INDIRECTLY RE-ENTER ANY WATER BODY OR WETLAND AREA.
- 6. TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED AS PER SPECIFICATIONS AND IN ACCORDANCE WITH NYS DOT STANDARD SHEETS, SECTION 209 AND WITHIN THE CONTRACT DOCUMENTS. THE COST OF MAINTAINING AND REMOVING TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INCLUDED IN THE BID PRICE OF THE APPROPRIATE ITEM USED FOR THE INSTALLATION OF THE MEASURE. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSPECTED BY THE CONTRACTOR DAILY AFTER EACH STORM EVENT THAT GENERATES RUNOFF. NOTED DEFICIENCIES SHALL BE CORRECTED WITHIN ONE BUSINESS DAY.
- 7. PROVIDE TEMPORARY STABILIZATION (SEED AND MULCH-TEMPORARY, ITEM 209.1003) FOR ALL DISTURBED AREAS AS SOON AS PRACTICABLE AND WITHIN 14-DAYS OF WHEN THE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED.

ITEM 698.06 STEEL/IRON PRICE ADJUSTMENT NOTES:

- 1. THE STEEL/IRON PRICE ADJUSTMENT ITEM IS INCLUDED IN THE CONTRACT TO PROTECT THE CONTRACTOR AND AUTHORITY FROM WIDE VARIATIONS IN THE COST OF STEEL DURING THE COURSE OF THIS CONTRACT.

WARNING: UNDERGROUND FIBER OPTIC CONDUIT:

- 1. THE THRUWAY'S FIBER OPTIC "BACKBONE" IS LOCATED WITHIN THE WORK LIMITS OF THIS PROJECT. THE APPROXIMATE LOCATION IS SHOWN ON THE DRAWINGS. THE CONTRACTOR IS ADVISED TO CONTACT UDIG NEW YORK AT 1-800-962-7962 (OR 811) PRIOR TO ANY EXCAVATION. FURTHERMORE, PURSUANT TO N.Y.S. CODE RULE 753, THE CONTRACTOR MUST BE PREPARED TO VERIFY THE LOCATION OF THE FIBER OPTIC LINE THROUGH HAND DUCT TEST HOLES AT ONE OR MORE LOCATIONS WITHIN THE WORK AREA PRIOR TO ANY EXCAVATION. HAND DUG TEST HOLES SHALL BE PAID FOR UNDER ITEM 206.05 - TEST PIT EXCAVATION (EACH) AS REQUIRED.

UTILITY NOTES:

- 1. LOCATION OF EXISTING UTILITIES, PUBLIC AND/OR PRIVATE, AS SHOWN IN THE PLANS OR INDICATED IN THE PROPOSAL ARE APPROXIMATE ONLY. THE EXACT LOCATION OF EACH UTILITY SHALL BE DETERMINED IN THE FIELD. ADDITIONAL UTILITY LINES NOT SHOWN ON THE PLANS, WHETHER ABANDONED OR IN SERVICE, MAY EXIST. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONDUCT THEIR OPERATIONS AND TAKE NECESSARY PRECAUTIONS TO PREVENT INTERFERENCE WITH OR DAMAGE TO THESE OR OTHER FACILITIES DURING THE COURSE OF CONSTRUCTION. THE CONTRACTOR IS ADVISED TO CONTACT UDIG NEW YORK AT 1-800-962-7962 (OR 811) PRIOR TO ANY EXCAVATION.
- 2. IN THE EVENT THE CONTRACTOR DAMAGES AN EXISTING UTILITY SERVICE CAUSING AN INTERRUPTION IN SAID SERVICE, THEY SHALL IMMEDIATELY COMMENCE WORK TO RESTORE SERVICE AND MAY NOT CEASE THEIR WORK OPERATION UNTIL SERVICE IS RESTORED.
- 3. THE METHOD OF REMOVAL OF EXISTING ROADWAY OR SHOULDER PAVEMENT IN THE IMMEDIATE VICINITY OF UNDERGROUND UTILITIES SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.
- 4. THE EXISTING OVERHEAD ELECTRIC THAT CROSSES THE MAILINE WILL BE RELOCATED BY NATIONAL GRID APPROXIMATELY 200 FEET EAST OF ITS CURRENT LOCATION. THIS WILL ALSO INVOLVE THE RELOCATION AND INSTALLATION OF SEVERAL UTILITY POLES. THE CONTRACTOR SHALL COORDINATE THEIR WORK WITH NATIONAL GRID INCLUDING SCHEDULING OF EXISTING SUPERSTRUCTURE REMOVAL OR ANY CONSTRUCTION ACTIVITY THAT REQUIRES THE USE OF A CRANE. THE CONTRACTOR SHALL PROVIDE CLEARING AND GRUBBING WITHIN THE HIGHWAY BOUNDARY, PAID FOR UNDER ITEM 201.06 TO FACILITATE THE UTILITY RELOCATION.

COORDINATION NOTES:

- 1. THE CONTRACTOR MAY BE REQUIRED TO COORDINATE THEIR WORK WITH OTHER CONTRACTORS AND AUTHORITY MAINTENANCE FORCES. THE CONTRACTOR SHALL SCHEDULE THEIR OPERATIONS SO AS TO CAUSE MINIMAL DISRUPTION TO TRAFFIC.

WORK TO BE DONE:

THE FOLLOWING IS A GENERAL DESCRIPTION OF WORK TO BE DONE UNDER THIS CONTRACT. THIS LIST IS INTENDED TO GIVE THE CONTRACTOR A GENERAL DESCRIPTION OF THE WORK INVOLVED AND IS NOT A COMPLETE LISTING OF ALL WORK TO BE DONE. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS EVEN THOUGH NOT SPECIFICALLY MENTIONED IN THIS LIST.

REPLACEMENT OF WESTBOUND STRUCTURE:

- 1. ESTABLISH AND PERFORM WORKZONE TRAFFIC CONTROL TO PERFORM UNDERDECK REPAIRS TO THE EASTBOUND STRUCTURE.
- 2. PERFORM UNDERDECK REPAIRS TO THE EASTBOUND STRUCTURE.
- 3. ESTABLISH AND PERFORM WORKZONE TRAFFIC CONTROL TO SHIFT TRAFFIC TO EASTBOUND STRUCTURE.
- 4. INSTALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES.
- 5. INSTALL INTERIM STEEL SHEETING AS DEPICTED IN THE CONTRACT PLANS.
- 6. REMOVE AND DISPOSE OF EXISTING SUPERSTRUCTURE. EXISTING GIRDERS TO BE REMOVED USING HOLD AND RELEASE (TRAFFIC CONTROL) METHOD ON ORISKANY BLVD. IN COORDINATION WITH LOCAL AUTHORITIES.
- 7. REMOVE AND DISPOSE OF EXISTING SUBSTRUCTURE:
 - a. PORTION OF THE EXISTING BEGIN AND END ABUTMENT.
 - b. PORTION OF THE EXISTING PIERS.
- 8. EXCAVATE FOR PROPOSED SUBSTRUCTURE (BEGIN AND END ABUTMENT).
- 9. DRIVE ABUTMENT AND WINGWALL PILES.
- 10. CONSTRUCT BEGIN AND END ABUTMENT STEM WALLS AND WINGWALLS.
- 11. ERECT STEEL GIRDERS USING HOLD AND RELEASE (TRAFFIC CONTROL) METHOD ON ORISKANY BLVD. IN COORDINATION WITH LOCAL AUTHORITIES.
- 12. CONSTRUCT BRIDGE DECK, BACKWALL, AND TOPSIDE FEATURES.
- 13. CONSTRUCT BEGIN AND END APPROACH SLAB AND SLEEPER SLABS.
- 14. RECONSTRUCT BEGIN AND END APPROACHES.
- 15. TIE-IN PROPOSED WORK WITH EXISTING. CUT AND MILL PORTION OF EXISTING PAVEMENT TO COMPLETE TIE-IN.

REPLACEMENT OF EASTBOUND STRUCTURE:

- 16. ESTABLISH AND PERFORM WORKZONE TRAFFIC CONTROL TO SHIFT TRAFFIC TO WESTBOUND STRUCTURE.
- 17. REPEAT STEPS 4 THROUGH 15 FOR THE EASTBOUND STRUCTURE.
- 18. INSTALL NEW MEDIAN BARRIER.
- 19. USING SHORT-DURATION LANE CLOSURES, INSTALL NEW PAVEMENT MARKINGS AND SIGNS AS SHOWN ON THE CONTRACT PLANS. REMOVE WORKZONE TRAFFIC CONTROL AND SPLIT TRAFFIC TO FINAL EASTBOUND AND WESTBOUND LAYOUT.
- 20. REMOVE TEMPORARY EROSION CONTROL MEASURES AFTER TURF ESTABLISHMENT.

CHECKED BY: D. LEVINE


DRAFTED BY: LTESNER

CHECKED BY: D. LEVINE

DESIGNED BY: J.P. O'LOUGHLIN

DESIGN SUPERVISOR: D. LEVINE

ALTERED ON:	AFFIXED ON: 05/08/2024
SIGNATURE: STAMP:	SIGNATURE: DANIEL A. LEVINE, PE STAMP:



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



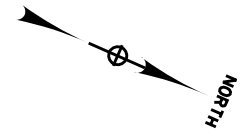


TITLE OF PROJECT REPLACEMENT OF I-90 BRIDGE OVER ORISKANY BOULEVARD	CONTRACT NUMBER: TAS 24-20B
LOCATION OF PROJECT SYRACUSE DIVISION MP 238.22	DATE: 04/03/2024
TITLE OF DRAWING GENERAL NOTES (SHEET 2 OF 2)	DRAWING NUMBER: GNN-2

M.P. 238.22, BIN 5009929

THIS SHEET SUPERSEDES SHEET 14

(PVMS) STANDARD SIZE
FULL MATRIX (LED)
APPROX. LOCATION
ITEM 619.110513



NOTES:

1. TRAFFIC ON ORISKANY BLVD. SHALL BE TEMPORARILY STOPPED FOR NO MORE THAN 10 MINUTES. AN EXCEPTION MAY BE MADE IN THE CASE OF AN EMERGENCY AS DETERMINED BY THE ENGINEER OR DIVISION TRAFFIC SUPERVISOR.
2. CONTRACTOR SHALL COORDINATE CLOSURES WITH NYS TROOP D
3. ONCE REOPENED TO TRAFFIC, THE ROADWAY SHALL NOT BE FULLY CLOSED AGAIN UNTIL ALL STOPPED TRAFFIC HAS CLEARED.
4. THE NEW YORK STATE POLICE AND THE MVTMC SHALL BE GIVEN SEVEN (7) CALENDAR DAYS NOTICE BY THE ENGINEER WHEN CONSTRUCTION OPERATIONS REQUIRE STOPPING TRAFFIC. THE CONTRACTOR SHALL SCHEDULE OPERATIONS ACCORDINGLY AND PROVIDE THE ENGINEER ADEQUATE NOTICE (MINIMUM OF 2 WEEKS). FAILURE OF THE CONTRACTOR TO PROVIDE ADEQUATE NOTICE TO THE ENGINEER WILL RESULT IN DELAYS TO THE CONTRACTOR'S SCHEDULED OPERATIONS.
5. ORISKANY BLVD. SHALL ONLY BE CLOSED FOR LIFTING AND LOWERING STEEL.
6. UTILIZE CLOSURE ON TC-26 TO REDUCE ORISKANY BLVD. TO ONE LANE IN EACH DIRECTION BEFORE IMPLEMENTING TEMPORARY STOPPAGES.
7. THE PORTABLE VARIABLE MESSAGE SIGNS SHALL BE LOCATED AS DIRECTED BY THE ENGINEER.
8. PORTABLE VARIABLE MESSAGE SIGN SHALL BE DEPLOYED AND MESSAGE DISPLAYED ONE WEEK IN ADVANCE OF TRAFFIC STOPPAGES TO ADVERTISE OF THE NIGHT WORK/STOPPAGES. PVMS DURING CLOSURE(S) SHOULD BE PLACED IN ADVANCE OF THE LANE CLOSURE TERMINI ON EACH SIDE OF BRIDGE. COORDINATE WITH NYS DOT SO MESSAGING FOR PVMS IS PLACED ON NYS SYSTEM (BOTH PRE-PHASE AND DURING PHASE MESSAGES).
9. STOPPAGE OF TRAFFIC WILL ONLY BE ALLOWED DURING THE HOURS OF 12:00AM TO 5:00AM.
10. THE CONTRACTOR SHALL PROVIDE FLAGGERS AS NEEDED TO ASSIST THE STATE POLICE IN TRAFFIC CONTROL.

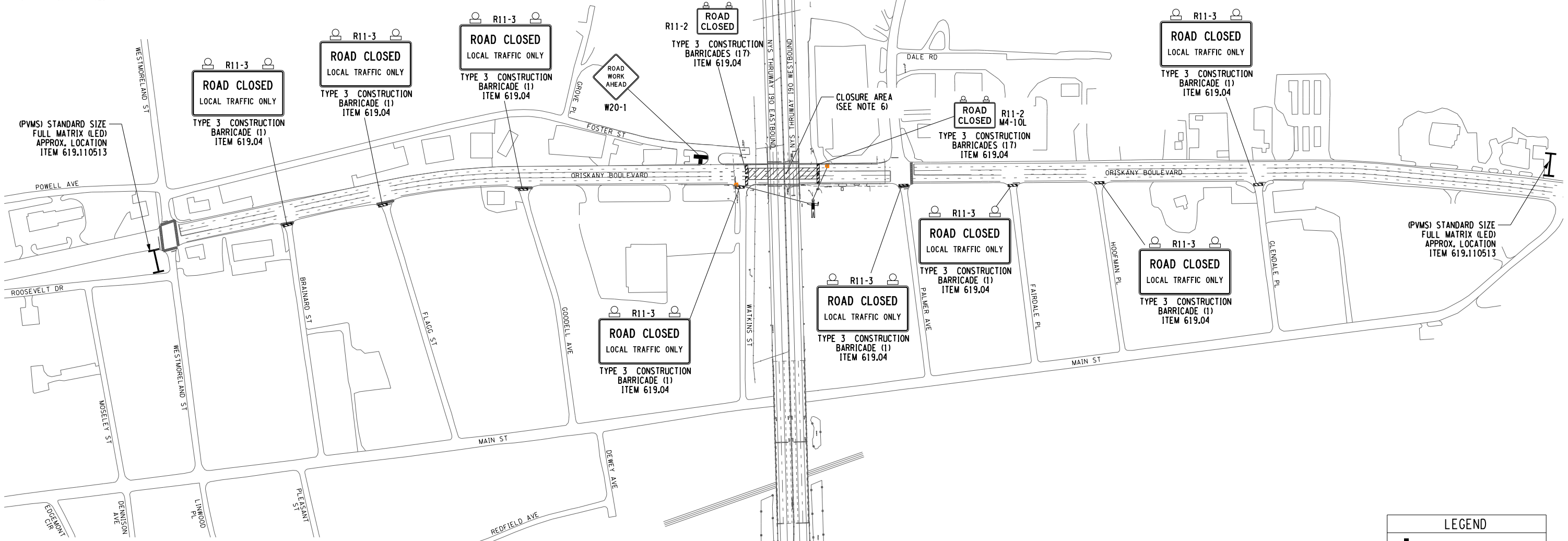
CHECKED BY: JMC

DRAFTED BY: DG

CHECKED BY: JMC

DESIGNED BY: GM

DESIGN SUPERVISOR: JMC



(PVMS) STANDARD SIZE
FULL MATRIX (LED)
APPROX. LOCATION
ITEM 619.110513

(PVMS) STANDARD SIZE
FULL MATRIX (LED)
APPROX. LOCATION
ITEM 619.110513

LEGEND	
	GROUND MOUNTED SIGN
	BARRICADE/SIGN LIGHTING
	TYPE III CONSTRUCTION BARRICADE WITH LIGHTING
	WORK SPACE

M.P. 238.22, BIN 5009929

ALTERED ON:	AFFIXED ON: 5/9/24
SIGNATURE: STAMP:	SIGNATURE: JAMES M. CUMMINGS, PE STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

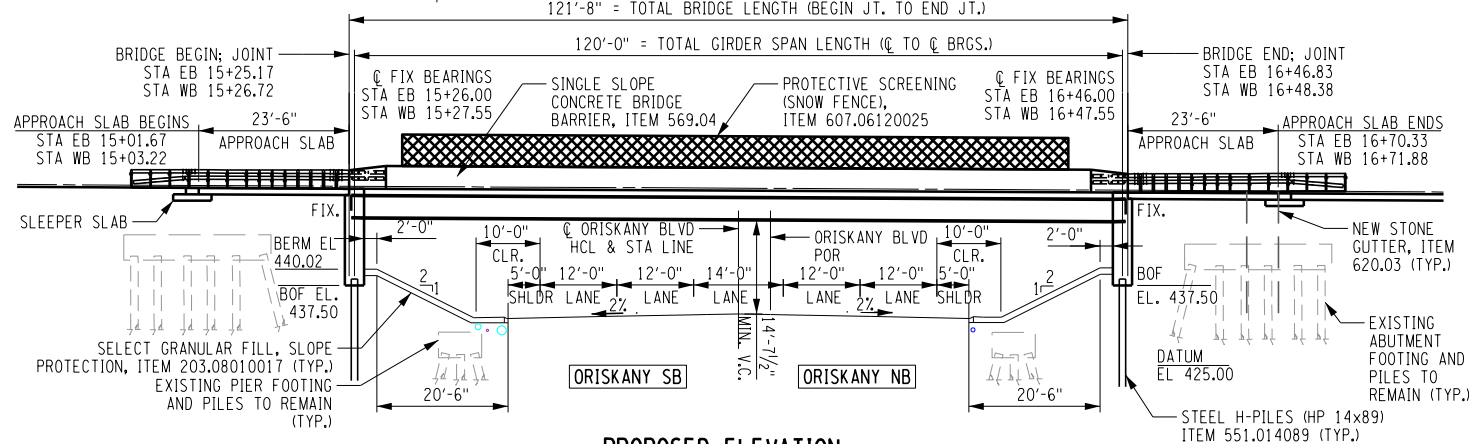
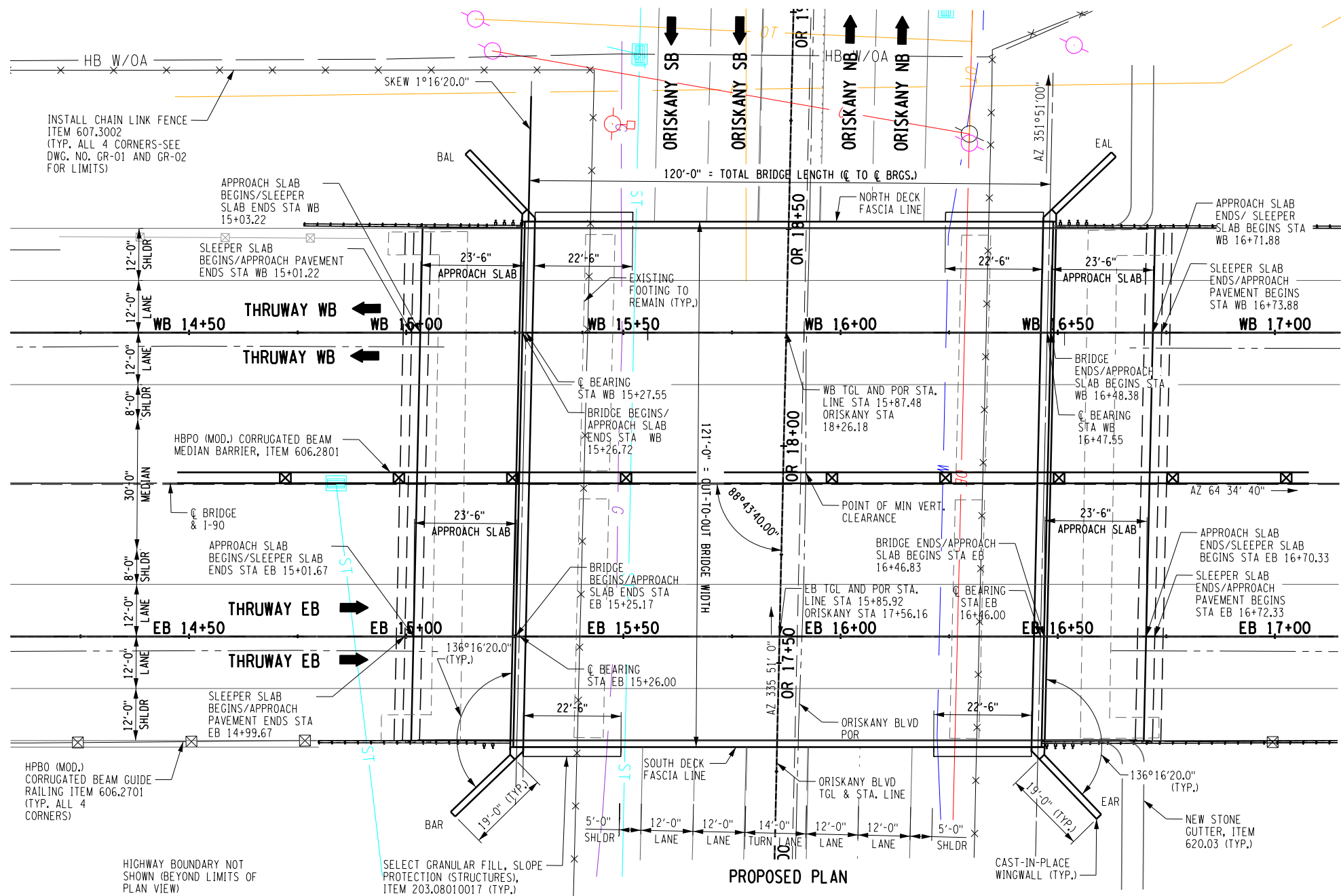
NEW YORK STATE OF OPPORTUNITY

Thruway Authority

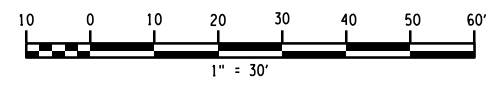
HUNT ENGINEERS | ARCHITECTS | SURVEYORS

HORSEHEADS, NY 607-358-1000 ROCHESTER, NY 585-327-7950
TOWANDA, PA 570-295-4888 BINGHAMTON, NY 607-736-8081
ALBANY, NY 607-736-8981 WWW.HUNT-ENG.COM
NY CERTIFICATE NO. 0018220 PA CERTIFICATE NO. TSC22031464-1

TITLE OF PROJECT REPLACEMENT OF I-90 BRIDGE OVER ORISKANY BOULEVARD	CONTRACT NUMBER: TAS 24-20B
LOCATION OF PROJECT SYRACUSE DIVISION MP 238.22	DATE: 04/03/2024
TITLE OF DRAWING TRAFFIC STOPPAGE PLAN	DRAWING NUMBER: TC-1



NOTES:
1. ORISKANY BOULEVARD (SR 69) IS UNDER THE JURISDICTION OF THE NYSOT, AND THE NYSOT SUPPORTS AND RECOMMENDS THAT A 10'-0" WIDE CLEAR ZONE BE USED UNDERNEATH THE NEW I-90 BRIDGE OVER ORISKANY BOULEVARD (SR 69). PER THE NYSOT, THE 10'-0" CLEAR ZONE DETERMINATION IS CONSISTENT WITH THE PREVAILING CLEAR ZONE OF 8 TO 10 FEET ALONG ORISKANY BOULEVARD (SR 69), THERE ARE NO EXISTING CRASH PATTERNS ON ORISKANY BOULEVARD (SR 69) IN THE VICINITY OF THE BRIDGE INDICATING THE NEED FOR A WIDER CLEAR ZONE, AND A 10'-0" WIDE CLEAR ZONE WOULD ALLOW FOR THE FUTURE CONSTRUCTION OF SIDEWALKS WITHOUT HAVING TO REGRADE THE SLOPES BETWEEN THE EXISTING CURBS AND PROPOSED ABUTMENTS.



CHECKED BY: D. LEVINE

DRAFTED BY: LITNESNER

CHECKED BY: D. LEVINE

DESIGNED BY: J.P. O'LOUGHLIN

DESIGN SUPERVISOR: D. LEVINE

ALTERED ON:
SIGNATURE:
STAMP:

AFFIXED ON: 05/08/2024
SIGNATURE: DANIEL A. LEVINE, PE
STAMP:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

Table with 4 columns: DATE, DESCRIPTION, BY, SYM. (REVISIONS)



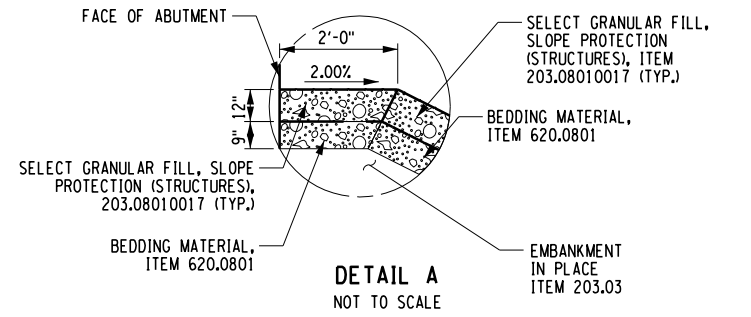
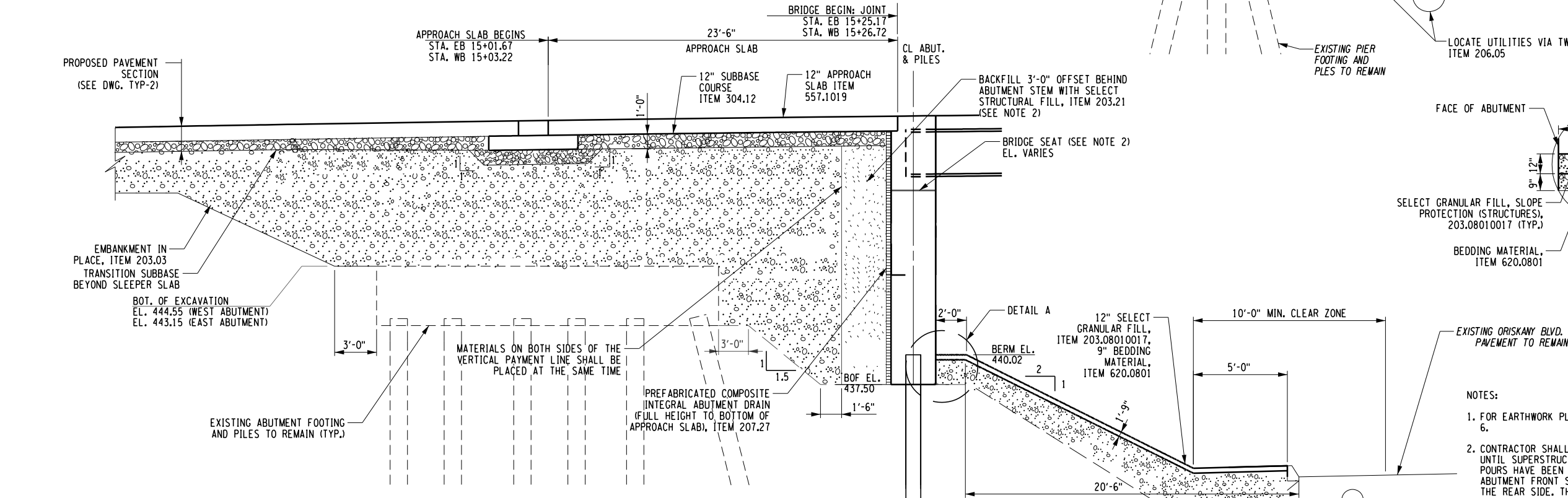
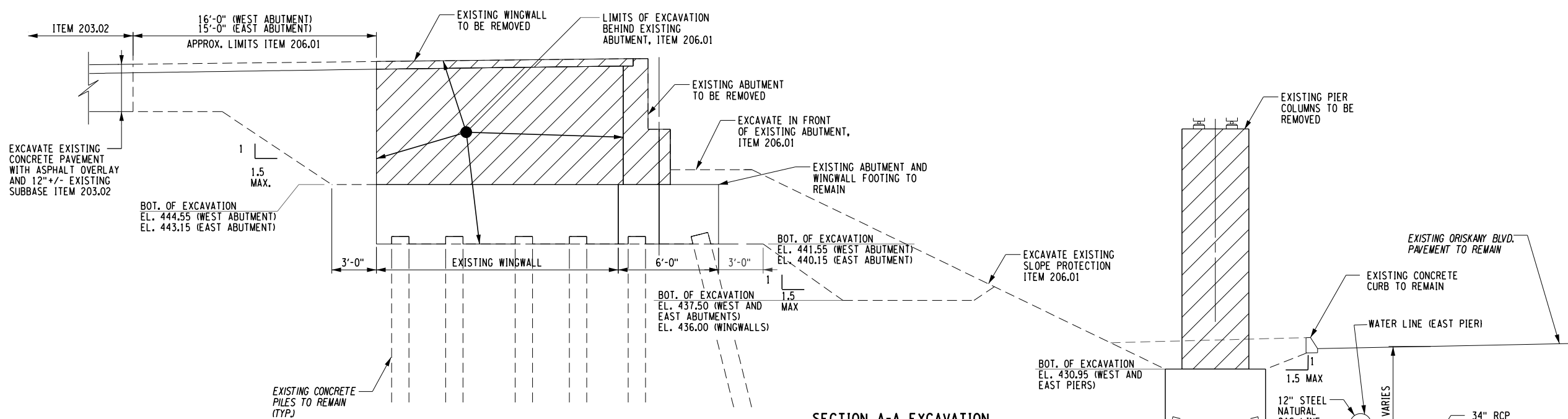
TITLE OF PROJECT: REPLACEMENT OF I-90 BRIDGE OVER ORISKANY BOULEVARD
LOCATION OF PROJECT: SYRACUSE DIVISION MP 238.22
TITLE OF DRAWING: PROPOSED BRIDGE PLAN AND ELEVATION

CONTRACT NUMBER: TAS 24-20B
DATE: 04/03/2024
DRAWING NUMBER: ST-2

M.P. 238.22, BIN 5009929

CHECKED BY: D. LEVINE
 DRAFTED BY: LTESNER
 CHECKED BY: D. LEVINE
 DESIGNED BY: J.P. O'LOUGHLIN
 DESIGN SUPERVISOR: D. LEVINE

LEGEND	
	REMOVAL OF SUBSTRUCTURE (ITEM 202.19)
	ENBANKMENT IN PLACE (ITEM 203.03)
	SELECT STRUCTURAL FILL (ITEM 203.21)
	SUBBASE STONE (ITEM 304.12)
	BEDDING MATERIAL (ITEM 620.0801)
	PAYMENT LINE FOR STRUCTURE EXCAVATION (ITEM 206.01)



- NOTES:
1. FOR EARTHWORK PLAN AND LOCATION OF SECTION A-A. SEE DWG. ST-5 AND ST-6.
 2. CONTRACTOR SHALL NOT BACKFILL ABOVE THE BRIDGE SEAT ELEVATION UNTIL SUPERSTRUCTURE HAS BEEN ERECTED AND THE DECK/BACKWALL POURS HAVE BEEN COMPLETED. ADDITIONALLY, BACKFILLING OF THE ABUTMENT FRONT SIDE SHALL BE PROGRESSED SIMULTANEOUSLY WITH THE REAR SIDE. THE MAXIMUM DIFFERENTIAL BETWEEN THE REAR AND FRONT SIDE BACKFILL SHOULD BE LIMITED TO 2'-0" +/-.
 3. ORISKANY BOULEVARD (SR 69) IS UNDER THE JURISDICTION OF THE NYS DOT, AND THE NYS DOT SUPPORTS AND RECOMMENDS THAT A 10'-0" WIDE CLEAR ZONE BE USED UNDERNEATH THE NEW I-90 BRIDGE OVER ORISKANY BOULEVARD (SR 69). PER THE NYS DOT, THE 10'-0" CLEAR ZONE DETERMINATION IS CONSISTENT WITH THE PREVAILING CLEAR ZONE OF 8 TO 10 FEET ALONG ORISKANY BOULEVARD (SR 69). THERE ARE NO EXISTING CRASH PATTERNS ON ORISKANY BOULEVARD (SR 69) IN THE VICINITY OF THE BRIDGE INDICATING THE NEED FOR A WIDER CLEAR ZONE, AND A 10'-0" WIDE CLEAR ZONE WOULD ALLOW FOR THE FUTURE CONSTRUCTION OF SIDEWALKS WITHOUT HAVING TO REGRADE THE SLOPES BETWEEN THE EXISTING CURBS AND PROPOSED ABUTMENTS.

M.P. 238.22, BIN 5009929

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REVISIONS			
DATE	DESCRIPTION	BY	SYL

PARSONS

TITLE OF PROJECT REPLACEMENT OF I-90 BRIDGE OVER ORISKANY BOULEVARD	CONTRACT NUMBER: TAS 24-20B
LOCATION OF PROJECT SYRACUSE DIVISION MP 238.22	DATE: 04/03/2024
TITLE OF DRAWING EXCAVATION AND EARTHWORK SECTIONS	DRAWING NUMBER: ST-7

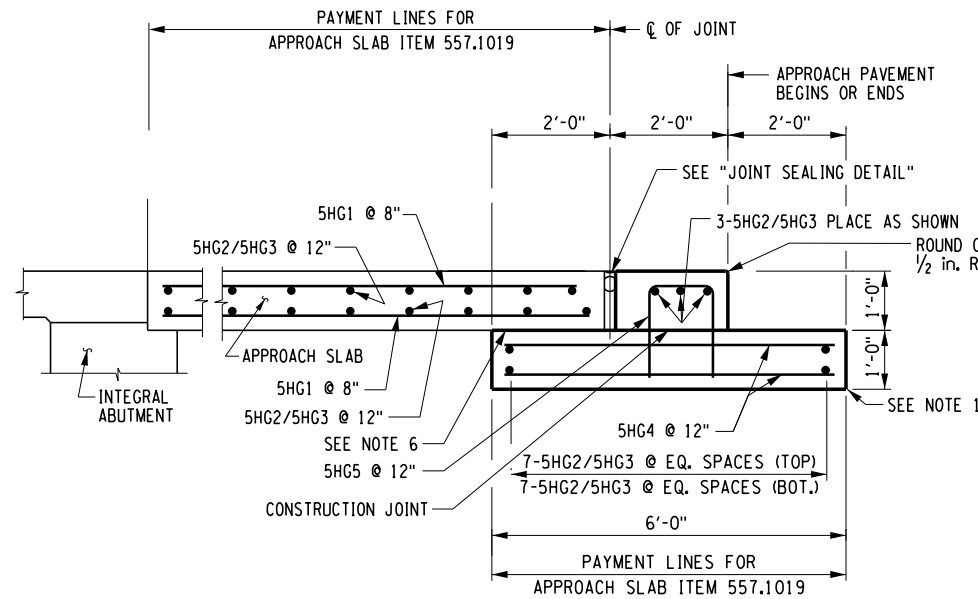
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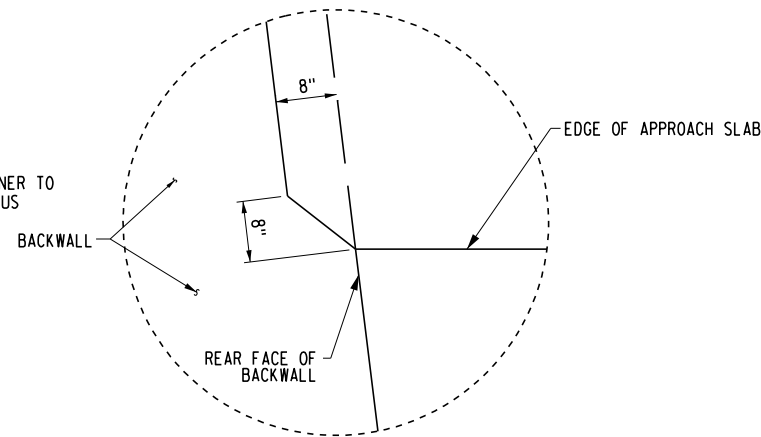
CHECKED BY: D. LEVINE

DESIGNED BY: J.P. O'LOUGHLIN

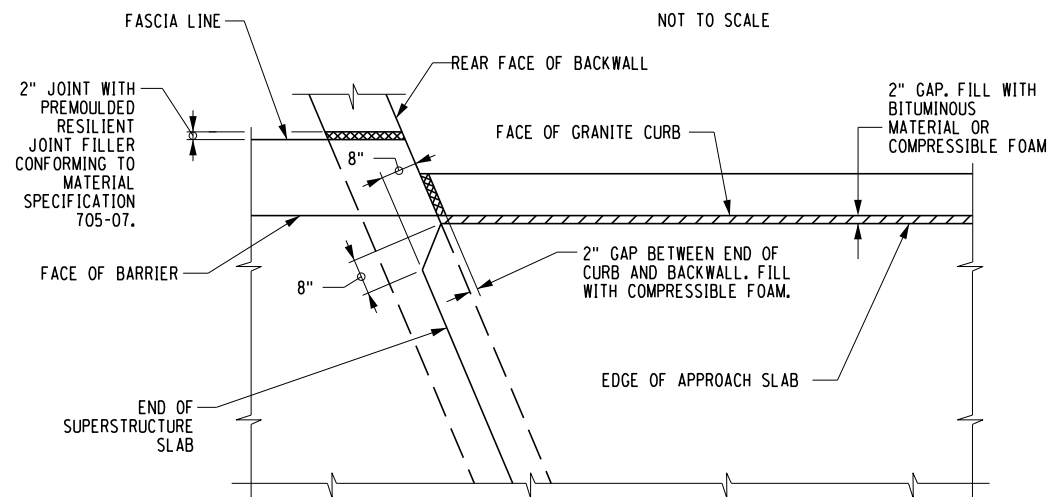
DESIGN SUPERVISOR: D. LEVINE



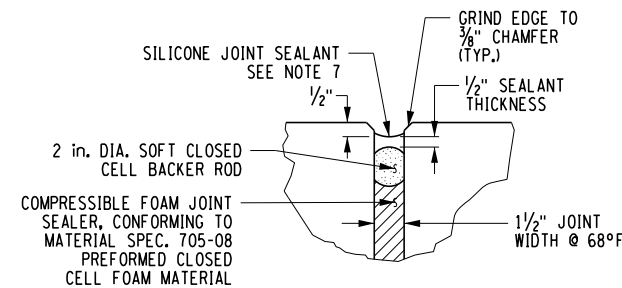
SLEEPER SLAB DETAIL



DETAIL "A"
NOT TO SCALE



APPROACH SLAB CORNER DETAIL
JOINTLESS / INTEGRAL BRIDGE WITH CONCRETE BARRIER




JOINT SEALING DETAIL
NOT TO SCALE

NOTES:

- EXCAVATION FOR SLEEPER SLABS SHALL BE CAREFULLY MADE AFTER COMPACTED ABUTMENT EMBANKMENT IS IN PLACE. THE SLEEPER SLABS SHALL BE FOUNDED ON UNDISTURBED COMPACT MATERIAL OR RE-COMPACTED MATERIAL. NO LOOSE BACKFILL SHALL BE ALLOWED. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT THE SLEEPER SLAB FROM TEMPORARY LOADINGS OR ANY CONDITION WHICH COULD CAUSE MOVEMENTS OR UNEVEN SETTLEMENT OF THE SLEEPER SLAB.
- TO PERMIT UNHINDERED LONGITUDINAL MOVEMENT OF THE APPROACH SLABS, THE SURFACE OF THE SUBBASE COURSE MUST BE ACCURATELY CONTROLLED TO FOLLOW AND BE PARALLEL TO THE ROADWAY GRADE AND CROSS SLOPE. TWO LAYERS OF POLYETHYLENE CURING COVERS (WHITE OPAQUE) IN ACCORDANCE WITH MATERIAL SUBSECTION 711-04 SHALL BE PLACED ON THE FINISHED SUBBASE COURSE FOR THE FULL WIDTH OF THE ROADWAY PRIOR TO PLACEMENT OF APPROACH SLAB REINFORCEMENT. THE CURING COVERS SHALL BE 4 MILS THICK, AND LAPS (PLACED PARALLEL TO THE LONGITUDINAL AXIS OF THE BRIDGE) SHALL BE 2 FT. MIN. COST TO BE INCLUDED IN THE UNIT PRICE BID FOR APPROACH SLABS, ITEM 557.1019.
- TOP OF SLEEPER SLABS SHALL BE STEEL TROWEL FINISHED AND COATED WITH A 0.04 in. NOMINAL THICKNESS OF PERFORMANCE GRADE ASPHALT AS INDICATED IN THE PROPOSAL, OR MATERIAL SPECIFICATION 702-3101. THE TOP OF SLEEPER SLABS SHALL FOLLOW THE CROSS SLOPE AND GRADE OF ROADWAY. COST TO BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROACH SLAB ITEM.
- THE TOP SURFACES OF THE STRUCTURAL SLABS, APPROACH SLABS, AND SLEEPER SLABS SHALL BE GROOVED PER ITEM 558.02, LONGITUDINAL SAW CUT GROOVING OF STRUCTURAL SLAB SURFACE. GROOVES SHALL BE CUT LONGITUDINALLY TO THE ROADWAY CENTERLINE. GROOVES SHALL EXTEND TO WITHIN NO CLOSER THAN 4" FROM FACE OF CURB OR 4" TO THE EDGE OF A SLAB, AND NO FURTHER THAN 15" FROM THE EDGE OF ANY SLAB.
- APPLY PROTECTIVE SEALER ITEM 559.01 TO ALL EXPOSED CONCRETE SURFACES, SEALER SHALL BE PENETRATING TYPE.
- ALL REINFORCEMENT SHALL HAVE 3 IN. COVER UNLESS OTHERWISE NOTED.
- FILL THE RECESS WITH A STRUCTURAL JOINT MATERIAL SEALANT, FROM THE DEPARTMENT'S APPROVED LIST. THE MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED FOR THE SEALANTS THAT REQUIRED A PRIMER. THE CONCRETE SHALL CURE FOR MINIMUM OF 7 DAYS BEFORE JOINT IS SEALED. SEALING SHALL BE PERFORMED WHEN THE CONCRETE TEMPERATURE IS 40°F OR ABOVE, BOTH JOINT FACES SHALL BE SAND BLASTED TO ROUGHEN THE SURFACE AND TO REMOVE ALL SURFACE MOISTURE AND ANY OTHER MATERIAL THAT MAY INTERFERE WITH BOND.

NOTES:
APPROACH RAILING NOT SHOWN
REINFORCEMENT NOT SHOWN

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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL



TITLE OF PROJECT REPLACEMENT OF I-90 BRIDGE OVER ORISKANY BOULEVARD	CONTRACT NUMBER: TAS 24-20B
LOCATION OF PROJECT SYRACUSE DIVISION MP 238.22	DATE: 04/03/2024
TITLE OF DRAWING APPROACH SLAB SECTIONS AND DETAILS (SHEET 3 OF 3)	DRAWING NUMBER: ST-49

M.P. 238.22, BIN 5009929

THIS SHEET SUPERSEDES SHEET 112

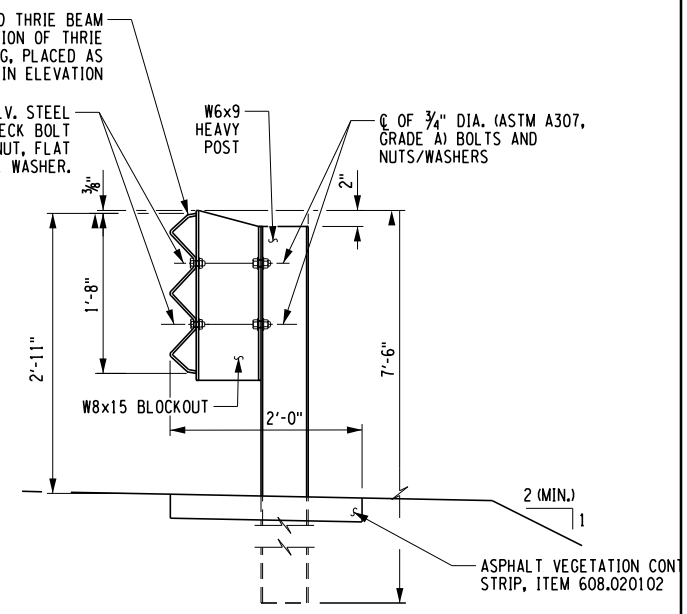
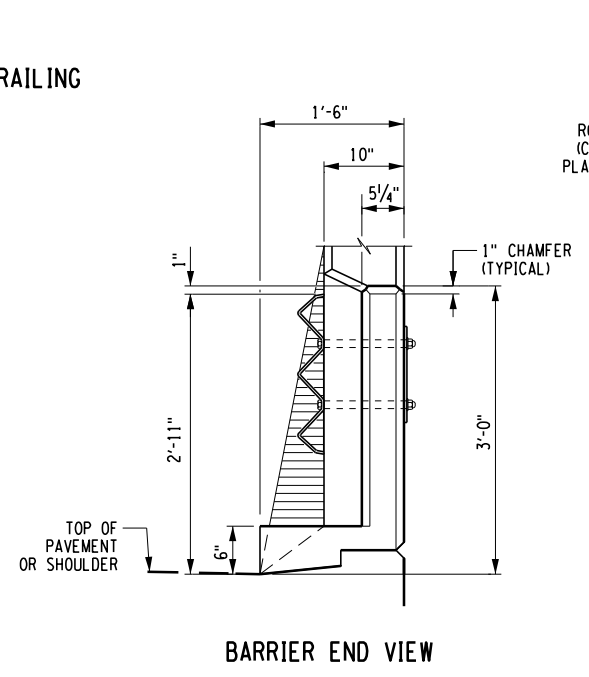
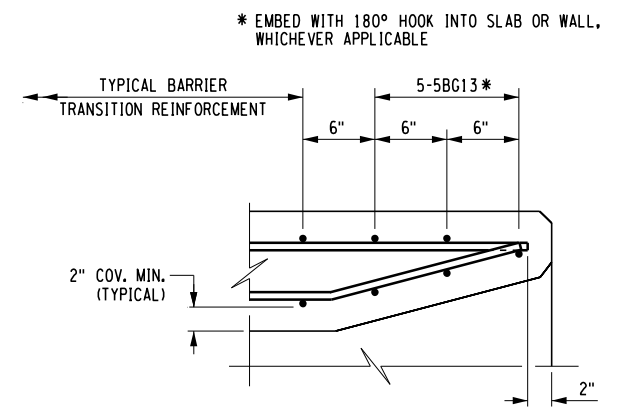
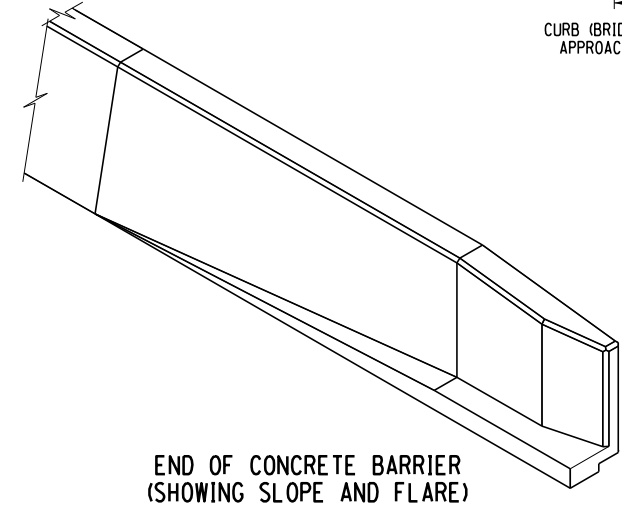
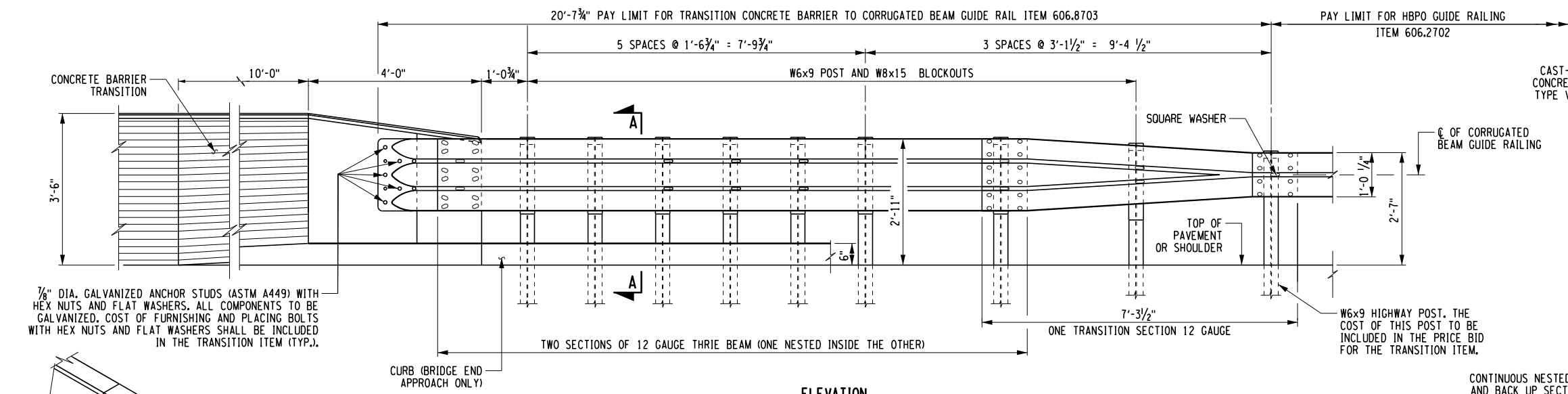
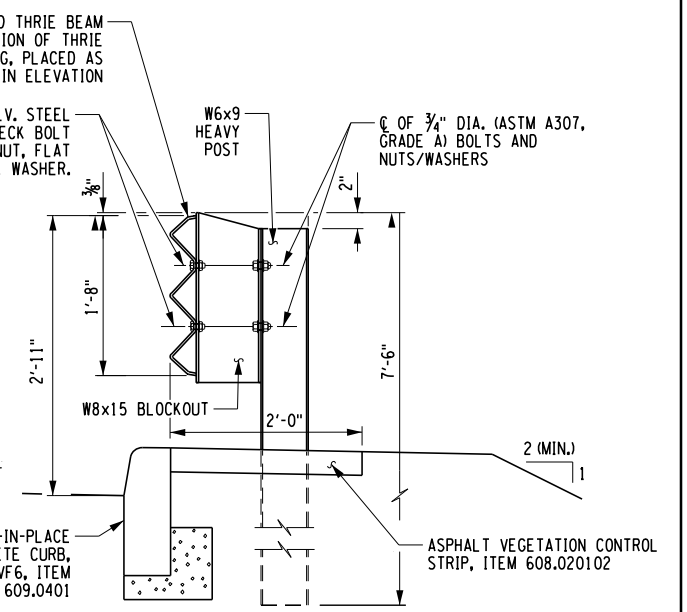
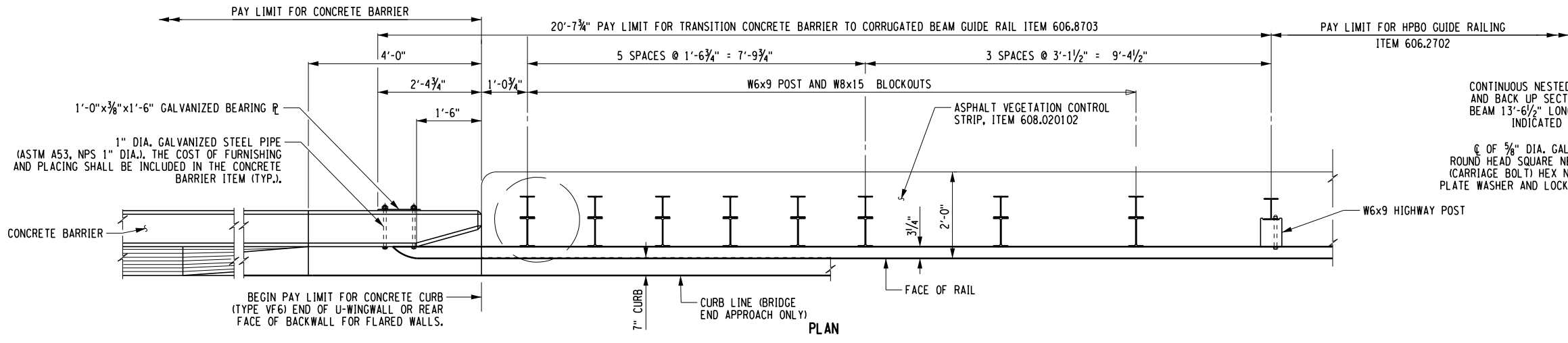
CHECKED BY: D. LEVINE

DRAFTED BY: LITNESNER

CHECKED BY: D. LEVINE

DESIGNED BY: J.P. O'LOUGHLIN

DESIGN SUPERVISOR: D. LEVINE



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LOCATION OF PROJECT SYRACUSE DIVISION MP 238.22	DATE: 04/03/2024
TITLE OF DRAWING APPROACH TRANSITION RAILING DETAILS (1 OF 2)	DRAWING NUMBER: ST-53